



AGENDA

ASTORIA PLANNING COMMISSION

December 17, 2014

6:30 p.m.

2nd Floor Council Chambers

1095 Duane Street • Astoria OR 97103

1. CALL TO ORDER
2. ROLL CALL
3. MINUTES
 - a. November 25, 2014 Work Session
4. PUBLIC HEARINGS
 - a. Permit Extension for Conditional Use CU03-04 by Elisabeth Nelson to request a one year extension for a temporary use, to August 26, 2015, to operate the Astoria Conservatory of Music in the existing church structure at 1103 Grand Avenue in the R-3, High Density Residential zone. Staff recommends approval of the request with conditions.
 - b. Conditional Use CU14-14 by Michelle Green to continue use of the existing drive-through coffee shop as a temporary use for one year at 230 37th Street in the S-1, Marine Industrial Shorelands zone. Staff recommends approval of the request with conditions.
5. REPORT OF OFFICERS
6. ADJOURNMENT TO WORK SESSION
 - a. Riverfront Vision Plan – Bridge Vista Area

THIS MEETING IS ACCESSIBLE TO THE DISABLED. AN INTERPRETER FOR THE HEARING IMPAIRED MAY BE REQUESTED UNDER THE TERMS OF ORS 192.630 BY CONTACTING SHERRI WILLIAMS, COMMUNITY DEVELOPMENT DEPARTMENT, 503-338-5183.

ASTORIA PLANNING COMMISSION

WORK SESSION – RIVERFRONT VISION, BRIDGE VISTA AREA SITE VISIT

NOVEMBER 25, 2014

2:00 P.M.

CALL TO ORDER

President Nemlowill called the meeting to order at 2:01 p.m.

ROLL CALL - ITEM 2:

Commissioners Present: President Zetty Nemlowill, Vice-President McLaren Innes, Pete Gimre, Dave Pearson, Kent Easom, Sean Fitzpatrick

Commissioners Excused: Thor Norgaard

Staff Present: City Manager Brett Estes, Planner Rosemary Johnson and Secretary Sherri Williams

Consultant: Matt Hastie, Angelo Planning Group

City Manager Estes explained the purpose of the work session site visit was for Commissioners to see the Bridge Vista area and get an idea of the building heights along the shoreline both in and out of the water. [Commissioners walked from the foot of 2nd Street to the Maritime Memorial and back.]

As Commissioners walked the River Trail, beginning at the foot of 2nd Street, Planner Johnson informed the Commissioners of the heights of the existing buildings and distances out into the water from the shoreline. She noted there is no vehicle access from West Marine Drive between the foot of 2nd Street to Columbia Avenue. In order to gain vehicle access, easements would be required. Upland property owners may have the water rights if they have permits from Division of State Lands (DSL). It was mentioned as there is no vehicle access in this area, it would be a good reason to have a 'no build' area at that location.

- Columbia House Condos (1 3rd Street) extend approximately 250' out into the water and approximately 50' high.
- From the shoreline to the historic cannery boiler is approximately 85'.
- Astoria Warehouse's building (70 W Marine) at the water's edge is approximately 125' deep x 160' wide. The stack of pallets is approximately 360' out into the River.
- Holiday Inn Express (204 W Marine) is approximately 45' high.
- Cannery Pier Hotel (10 Basin) is approximately 500' out into the water. The hotel is 46'-48' high. The Boat House is approximately 160' from the shoreline.

Misc.

- The Commission should come to a consensus of where the distance out should be calculated as the shoreline fluctuates. It was suggested that the City rail property line could be used.
- The area at the Maritime Memorial (10 Bay) should be a protected view area.
- The grassy area southwest of the Maritime Memorial is partially owned by the Port of Astoria and how this parcel is zoned for development should be discussed.
- From Columbia Avenue at the Pig-n-Pancake (146 W Bond) west to the Dunes Motel (288 W Marine) is more of a pedestrian area. The buildings are constructed at the street with coffee shops, restaurants, etc.
- Discussed how zone changes can make an area more pedestrian and family friendly.
- The Codes and zones in the 'Bridge Vista' area were last updated in 1982 with the exception of a couple of spot changes in the zoning.

There being no further business, the work session adjourned at 2:49 p.m.

ATTEST:

APPROVED:

Secretary

City Manager

STAFF REPORT AND FINDINGS OF FACT

December 8, 2014

TO: ASTORIA PLANNING COMMISSION

FROM: ROSEMARY JOHNSON, PLANNER



SUBJECT: PERMIT EXTENSION REQUEST ON CONDITIONAL USE PERMIT (CU03-04)
BY ELISABETH NELSON / ASTORIA CONSERVATORY OF MUSIC AT 1103
GRAND AVENUE

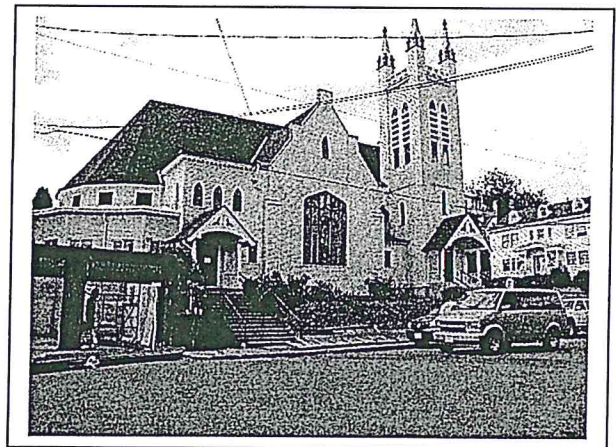
I. BACKGROUND SUMMARY

- A. Applicant: Elisabeth Nelson
Astoria Conservatory of Music
PO Box 81
Astoria OR 97103
- B. Owner: First Presbyterian Church
1103 Grand
Astoria OR 97103
- C. Location: 1103 Grand Avenue; Map T8N-R9W Section 8CD, Tax Lot 5700;
Lots 1, 2, 13, 14, Block 91, McClure's
- D. Zone: R-3 (High Density Residential)
- E. Permit: To locate a school of music as a Temporary Use in an existing
building; approved August 26, 2003
- F. Request: For a one year extension to August 26, 2015

II. BACKGROUND

A. Subject Property

The subject property is located within the R-3 (High Density Residential), on the south side of Grand Avenue at 11th Street. The building is currently used as the First Presbyterian Church and associated offices and class rooms. The adjacent church hall is located on Harrison Avenue and 11th and is used by an aerobics class.



B. Original Permit.

At its August 26, 2003 meeting, the Astoria Planning Commission (APC) approved a Conditional Use Permit (CU03-04) by Elisabeth Nelson / Astoria Conservatory of Music to locate the music school as a temporary use in the existing church facility. The Findings of Fact and conditions as approved on August 26, 2003 are hereby incorporated as part of this document.

At its November 26, 2014 meeting, the APC approved a one year extension on the permit to August 26, 2014.

The Development Code previously stated that a permit was valid for one year unless an extension was granted. In a LUBA decision for Astoria, it was determined that extensions were limited to only one extension. On April 19, 2010, the City Council amended the Permit Extension portion of the Code to allow a permit to be initially valid for two years with multiple extensions and with no maximum for the number of years a temporary use permit would be valid. Permits approved prior to adoption of the Code are subject to the amended Code.

III. PUBLIC REVIEW AND COMMENT

A public notice was mailed to all property owners within 100 feet pursuant to Section 9.020 on November 24, 2014 and to parties on the Record pursuant to Section 9.100.B.3.b. A notice of public hearing was published in the Daily Astorian on December 10, 2014. Any comments received will be made available at the Planning Commission meeting.

IV. APPLICABLE REVIEW CRITERIA AND FINDINGS OF FACT

- A. Development Code Section 3.240.A, Duration of Permits on Temporary Use, states that

- "1. A temporary use permit shall expire one year from the date of Final Decision unless an extension has been granted.*
- 2. Prior to permit expiration, the applicant may request extensions in accordance with Section 9.100(B.2.a & b) and 9.100(B.3 & 4). A permit remains valid, if a timely request for extension has been filed, until an extension is granted or denied."*

Finding: The original permit was issued on August 26, 2003 under the previous Code on permit extensions. The permit has received one extension each year with the most recent extension to August 26, 2014. An application for extension was received on August 22, 2014 prior to the permit expiration and therefore the permit is still valid pending review of this request.

- B. Section 9.100.B.1.c, Permit Extensions, states that *"One year extensions may be granted in accordance with the requirements of this Section as follows:*

1. Permit Extension Time Limit.

- c. *No more than three permit extensions may be granted. No variances may be granted from this provision. Temporary Use Permit extensions are exempt from this requirement and may exceed the three extensions limitation."*

Finding: The permit is for a Temporary Use and therefore is not subject to the maximum number of permit extensions.

C. Development Code Section 9.100.B.2, Permit Extension Criteria states that *"The granting authority may grant a permit extension upon written findings that the request complies with the following:*

- a. *The project proposal has not been modified in such a manner as to conflict with the original findings of fact for approval; and"*

Finding: No major changes have been made to the original approved project. This criteria is met.

- "b. *The proposed project does not conflict with any changes to the Comprehensive Plan or Development Code which were adopted since the last permit expiration date; and"*

Finding: The Comprehensive Plan has been amended relative to formatting and the Buildable Lands Inventory but would not impact the proposed project approval. The Development Code sections concerning permit extensions have been amended but would not impact the proposed project approval. No other Development Code sections have been amended that are relevant to this project. This criteria is met.

Finding: The application meets the criteria to allow a one year extension to August 26, 2015.

D. Development Code Section 9.100.B.3 & 4 concerning Permit Extensions states that

"3. *Permit Extension Procedures*

- a. *Applications for permit extensions shall be submitted in accordance with the Administrative Procedures in Article 9. Permit extension requests shall be submitted to the Community Development Department prior to permit expiration.*
- b. *Public notice and procedures on applications for permit extension requests shall be in accordance with the Administrative Procedures in Article 9. However, in addition to mailed notice as required in Article 9, notice shall be provided also to those on the*

record for the original permit, associated appeals, and associated extensions.

- c. *The Administrative decision, public hearing, and/or Commission/Committee decision concerning a permit extension may occur after the permit would have expired but for a timely filed request for a permit extension.*

4. Appeals.

The decision concerning a permit extension may be appealed. Appeals shall be made in accordance with Administrative Procedures in Article 9. Appeals on permit extensions shall be limited to the issues relevant to the permit extension criteria only and not to issues relevant to the original permit approval."

Finding: The applicant applied for the extension on August 22, 2014 prior to the expiration of the permit. Notices were mailed as noted in Section II above. The original permit was not appealed.

V. CONCLUSION

The request, in balance, meets all the applicable review criteria. Staff recommends approval of the request with the following conditions:

1. The Findings of Fact and conditions as approved on August 26, 2003 shall remain applicable to this permit extension.

The applicant should be aware of the following requirement:

The applicant shall obtain all necessary City and building permits prior to the start of construction.



CITY OF ASTORIA
Founded 1811 • Incorporated 1856
COMMUNITY DEVELOPMENT

revd 8/22/14

pd.

Original Permit No. CU03-04

Fee: \$100.00

PERMIT EXTENSION & RENEWAL APPLICATION

Property Address: 1103 Grand

Lot 1-2-13-14 Block 91 Subdivision McChure

Map 8CD Tax Lot 5700 Zone R-3

Applicant Name: Elisabeth Nelson

Mailing Address: P.O. Box 81, Astoria, OR 97103

Phone: 503-298-0338 Business Phone: 503-325-3237 Email: lisa@astoriamusic.com

Property Owner's Name: First Presbyterian Church of Astoria

Mailing Address: 1103 Grand Ave., Astoria, OR 97103

Business Name (if applicable): Astoria Conservatory of Music

Signature of Applicant: Elisabeth Nelson Date: 8/22/14

Signature of Property Owner: _____ Date: _____

Approved Permit to be Extended: CU03-04

Date of Original Approval: 8-26-03

Proposed One Year Extension Date: 8-26-15

Reason Extension is Required: (Also address criteria listed on second page of this application)

Continued use

one year extension to 8-26-15 for cond use. permit CU03-04

FILING INFORMATION: Planning Commission meets on the fourth Tuesday of each month. Historic Landmarks Commission meets on the third Tuesday of each month. Complete applications must be received by the 13th of the month to be on the next month's agenda. A Pre-Application meeting with the Associate Planner is required prior to acceptance of the application as complete. Only complete applications will be scheduled on the agenda. Your attendance at the Commission meeting is recommended.

operate a school of music in an existing comm bldg.

| For office use only: | | | |
|-----------------------|----------------|--------------------------|---------------------|
| Application Complete: | | Permit Info Into D-Base: | <u>11-5-14</u> |
| Labels Prepared: | <u>11-5-14</u> | Tentative Meeting Date: | <u>12/23/14</u> (3) |
| 120 Days: | | | |

STAFF REPORT AND FINDINGS OF FACT

December 10, 2014

TO: ASTORIA PLANNING COMMISSION

FROM: ROSEMARY JOHNSON, PLANNER



SUBJECT: CONDITIONAL USE REQUEST (CU14-14) BY MICHELLE GREEN TO
CONTINUE OPERATION OF AN ESPRESSO BUILDING AT 230 37th STREET
AS A TEMPORARY USE

I. BACKGROUND SUMMARY

- A. Applicant: Michelle Green
93079 Scandinavian Road
Astoria OR 97103
- B. Owner: Port of Astoria
10 Pier 1 #103
Astoria OR 97103
- C. Location: 230 37th Street; Map T8N-R9W Section 9AC, Tax Lot 100; Lots 1 & 8, and east 25' Lots 2 & 7, Block 1, Adairs Port of Upper Astoria, and vacated portion of alley
- D. Proposal: To continue operation an existing espresso building with drive-through as a temporary use
- E. Zone: S-1 (Marine Industrial Shoreland)

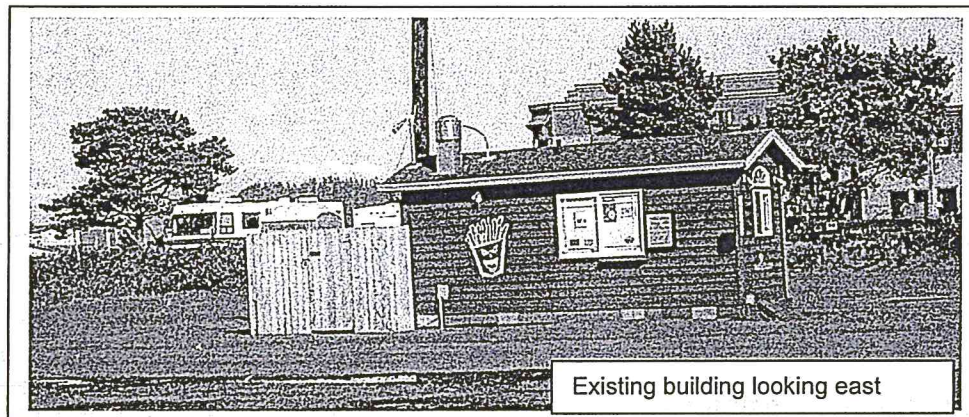
II. BACKGROUND

A. Subject Property

The subject property is located within the S-1 Zone (Marine Industrial Shoreland), on the north side of Lief Erikson Drive at 37th Street. The site is developed with a previous espresso stand which was approved as a temporary use through Conditional Use permits CU03-01 in 2003, CU06-03 in 2006, CU09-07 in 2009, and CU12-05 in 2012. In each case, the use ceased and the permit expired and was not extended, however the building remained on the site with the business listed for sale. Since the owner did not renew the temporary use and the permit expired a new conditional use review rather than a just a renewal is required.

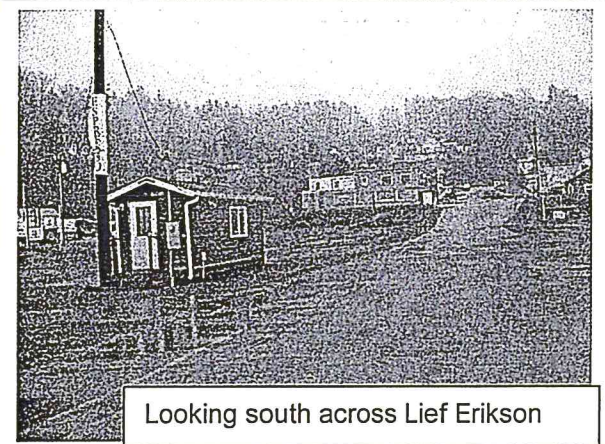
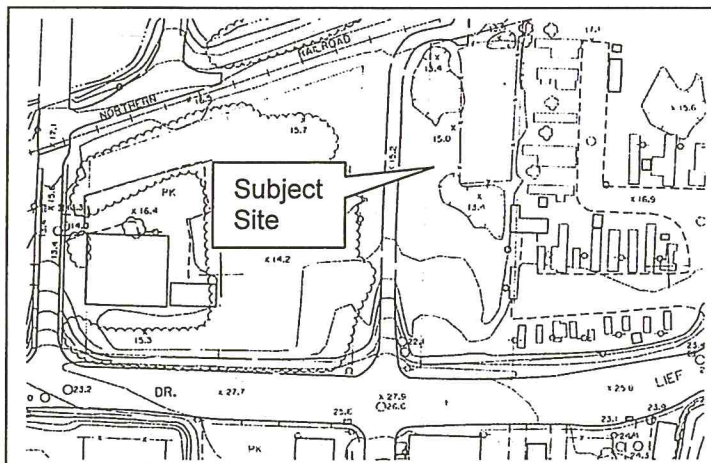
The design and location of the structure on this lot were reviewed and approved by the Historic Landmarks Commission (HLC) on May 16, 2006 and no changes are proposed to the building that would require additional HLC review.

The site is currently developed with a 160 square foot espresso stand and has been underutilized for many years. The subject property is approximately 38,000 square feet (0.87 acres) of land area; however, the applicant will only be leasing an approximate 70' x 35' (2,450 square feet) portion of the northern part of the lot. The site is a rectangle shape with 70' of frontage on 37th Street which is set back 240' from Lief Erikson Drive. The northern portion of the site is flat with a rise of approximately 30' toward Lief Erikson Drive. The 37th Street right-of-way is paved to a width of approximately 18' with no curbs or sidewalks. The site is partially graveled and contains a concrete pad. The City and ODOT made intersection improvements at 37th Street and Lief Erikson Drive which included improvements to pedestrian access along 37th Street.



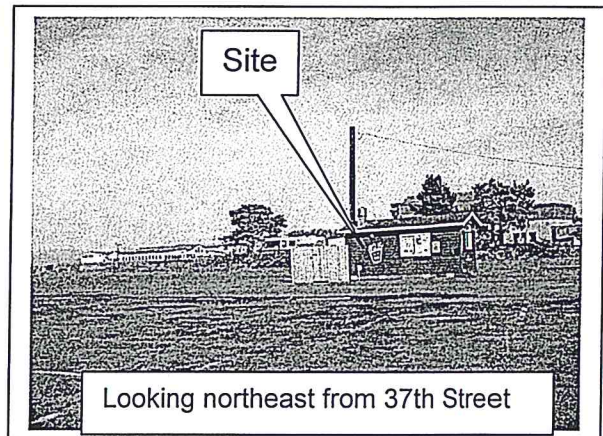
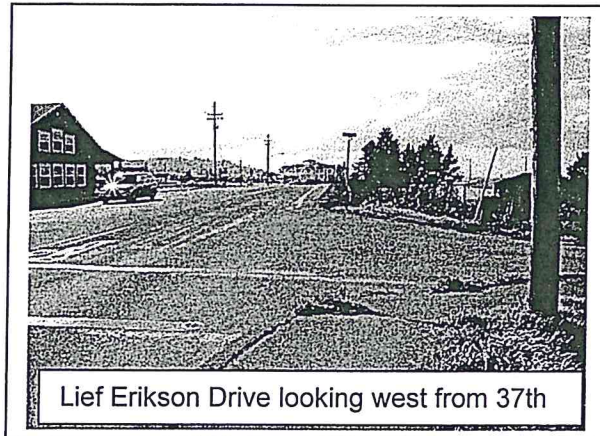
B. Adjacent Neighborhood

The site is bounded on the north by the City railroad line, on the south by Lief Erikson Drive, on the east by former Lovvold Trailer Court, and on the west by a vacant lot owned by the Port adjacent to the animal clinic building formerly the MTC building and OSU Seafood Lab.



This area of Lief Erikson Drive has a mixture of development. Both sides of 37th Street are vacant lots. To the east, there is a former trailer court which is developed from the City railroad tracks to the Lief Erikson Drive right-of-way. Across Lief Erikson Drive, is Geno's Pizza, the Columbia Ball Fields, the Astoria

Coffee Company (historic building), and two residences (one of which is historic and is currently used as retail sales).



C. Proposed Use

The applicant proposes to continue operation of an espresso stand with drive through window at this site as a temporary use on a year to year basis. On April 22, 2003, the APC approved Conditional Use Permit (CU03-01) to Jody and Todd Flukinger to locate the espresso stand at this location. The operation continued at that location until it closed in early 2006 and the permit had been allowed to lapse in 2005. In 2006, a new owner obtained a new Conditional Use permit (CU06-03) which received an extension through to May 23, 2008. The operation closed and the business was again listed for sale. In 2009, the new owner Michael Weissenfluh obtained a new Conditional Use permit (CU09-07) for a temporary use, but never applied for an extension. In 2012, Brian Reichert obtained Conditional Use permit (CU12-05) for temporary use to operate a drive through eating facility. Again, that use ceased and the permit expired. Since the former permit was not renewed within the allowable time limit, any continued or new use at this location would require a new conditional use permit and not just an extension of the previous permit. The applicant proposes to operate the same business as a drive through coffee shop.

The request is for a Temporary Use which is a Conditional Use in the S-1 Zone. Temporary Use is defined in the Development Code as *"A use or activity involving minimal capital investment that does not result in the permanent alteration of the site or construction of new buildings, and is removed from the site within one year, unless otherwise extended"*. There is an existing concrete slab at the subject location. The applicant is proposing to continue with the existing espresso stand on the site. The applicant has a month-to-month rental agreement with the Port of Astoria for use of the site.

III. PUBLIC REVIEW AND COMMENT

A public notice was mailed to all property owners within 100 feet pursuant to Section 9.020 on November 24, 2014. A notice of public hearing was published in the Daily

Astorian on December 10, 2014. Comments received will be made available at the Planning Commission meeting.

IV. APPLICABLE REVIEW CRITERIA AND FINDINGS OF FACT

- A. Section 2.660(5) identifies *"Temporary use meeting the requirements of Section 3.240"* as a use allowed conditionally in the S-1 Zone.

Finding: The applicant is proposing to operate an espresso stand as a Temporary Use in the S-1 Zone. Temporary uses meeting the requirements of Sections 3.240 is listed as a use allowed conditionally in the S-1 Zone.

- B. Section 2.665(1) states that *"All uses shall satisfy applicable Columbia River Estuary Shoreland and Aquatic Area Use and Activity Standards in Article 4."*

Section 4.160(1) Residential, Commercial and Industrial Development standards of the Columbia River Estuary and Shoreland Regional Standards states that *"sign placement shall not impair views of water areas. Signs shall be constructed against existing buildings whenever feasible. Off-premise outdoor advertising shall not be allowed in aquatic areas."*

Finding: Signage is not proposed with this application. A sign permit will be required for any signs at this location and will be reviewed against this Development Code standard and the Sign Code standards (Condition 3).

- C. Section 3.240(A) states that *"Temporary Uses are those which involve minimal capital investment, and which comply with the following standards: (A) Time Limit. Authorization for a temporary use shall be valid for a period of one (1) year after issuance. Prior to expiration, the applicant may request an extension. The Planning Commission, at its discretion, may extend authorization for an additional period up to one year. In extending a temporary use, the Planning Commission may impose any conditions which it deems necessary to protect the interests of the community."*

Finding: The use as proposed by the applicant involves minimal capital investment. The applicant is proposing to continue to operate the existing prefabricated espresso stand on the site. The mobility of the structure makes it possible to move at the termination of the permit or lease from the Port of Astoria. The applicant is aware that the temporary use is valid for 12 months, and that extension of the Temporary Use permit shall be in accordance with Section 9.020 except that it shall be mailed only to persons who provided written or oral testimony at the original public hearing (Condition 1).

- D. Section 3.240(B) states that *"Security. The Planning Commission may require that the applicant furnish the City with a performance bond or other negotiable instrument up to, and not to exceed, the value of the improvements or the cost of removal of the improvements, whichever is greater. This requirement may be made in order to assure that any conditions imposed are completed in accordance*

with the plans and specifications as approved by the Planning Commission, and the standards established in granting the use."

Finding: Security for improvements is not required as there are no proposed permanent improvements and the building can be removed at any time.

- E. Section 11.020(B)(1) concerning Conditional Uses Application and Procedures, requires that the proposed use comply with the applicable policies of the Comprehensive Plan.

1. Section CP.150(E.3) concerning Water-Dependent Development Shoreland states that *"Temporary uses involving an existing structure or involving new facilities requiring a minimal capital investment and no permanent structures"* is a permitted use.

Finding: The proposed use is temporary, requires minimal capital investment and there will not be a permanent structure as the building is removable.

2. Section CP.360(8) states that *"Access along arterial streets and highways will be limited to existing side streets wherever possible, or to common access points. The clustering of commercial uses will be employed in new, larger developments, and proper traffic control will be provided by the developer as the City Engineer may deem necessary. The District Engineer of the State Highway Division will be asked to comment on all new access proposals along State or Federal routes."*

Finding: Access to the proposed espresso stand will be from 37th Street, an existing side street and not directly from Lief Erikson Drive which is an arterial street. The proposed structure is a 160 square foot temporary structure and is not classified as a large development. It is the only building proposed to be located on the 15,400 square foot lot. The Oregon Department of Transportation (ODOT) Astoria and Salem offices were notified at the time the project was originally submitted. No comments were received from ODOT at that time. Since the new business is a continuation of a previous use, additional comment from the City Engineer was not submitted.

- F. Section 11.030(A), Basic Conditional Use Standards, requires that before a conditional use is approved, findings will be made that the use will comply with the following standards:

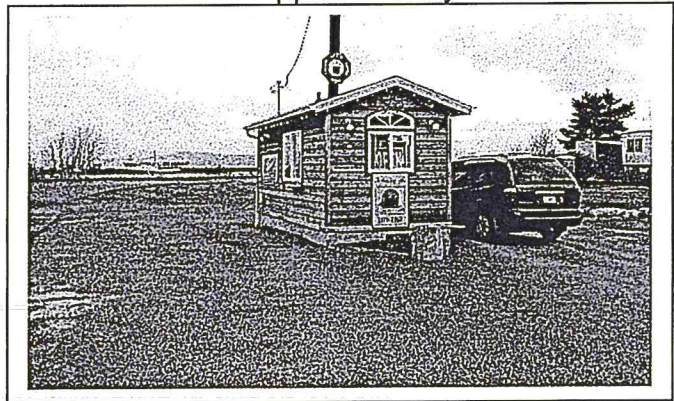
1. Section 11.030(A.1) requires that *"the use is appropriate at the proposed location. Several factors which should be considered in determining whether or not the use is appropriate include: accessibility for users (such as customers and employees); availability of similar existing uses; availability of other appropriately zoned sites; and the desirability of other suitably zoned sites for the use."*

Finding: The proposed use is appropriate as a Temporary Use at this location. The area was vacant and underutilized and is located on a side street easily accessible from Lief Erikson Drive. The site is a rectangle shape with 70' of frontage on 37th Street which is set back 240' from Lief Erikson Drive. The site has ample driving area for customers to access the building from both sides of the drive-through and continue to drive back onto 37th Street. The use will have little impact on the immediate site, as there will not be any permanent structure.

The original permit was the result of the business moving from a previous location at 3300 Lief Erikson Drive (Hauke Sentry Market) when the site was demolished for construction of the new Safeway store in 2003. Other zones such as General Commercial (C-3), are more appropriate if the use was proposed as a permanent (not temporary) use, however suitable real property in the C-3 Zone on the east side of Astoria is currently not available. Other sites on the west end of Astoria are not as desirable as the business has existing clientele that have been served on this site since 2003. Since the original stand opened at this site in 2003, two other espresso businesses have opened in the same general area, but do not have drive-through service. The increase in these type businesses in this area attests to the need for this service.

2. Section 11.030(A)(2) requires that *"an adequate site layout will be used for transportation activities. Consideration should be given to the suitability of any access points, on-site drives, parking, loading and unloading areas, refuse collection and disposal points, sidewalks, bike paths, or other transportation facilities. Suitability, in part, should be determined by the potential impact of these facilities on safety, traffic flow and control, and emergency vehicle movements."*

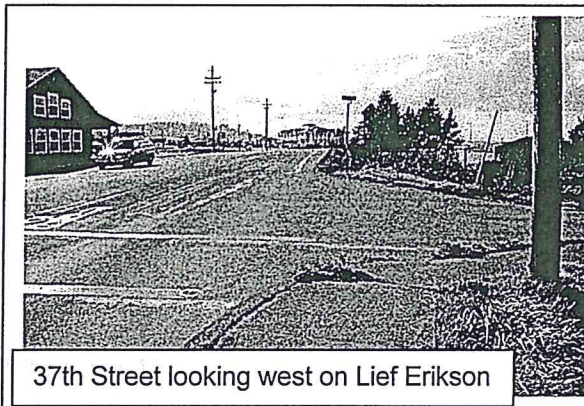
Finding: The site is 70' x 35' and the site plan indicates ample room for the drive-through loop and access to and from the site from 37th Street. The northern portion of the site is flat with a rise of approximately 30' toward Lief Erikson Drive. The site is partially graveled, has a concrete pad, and there is a graveled level driving area. The site is easily accessed off of Lief Erikson Drive which is an arterial street in Astoria.



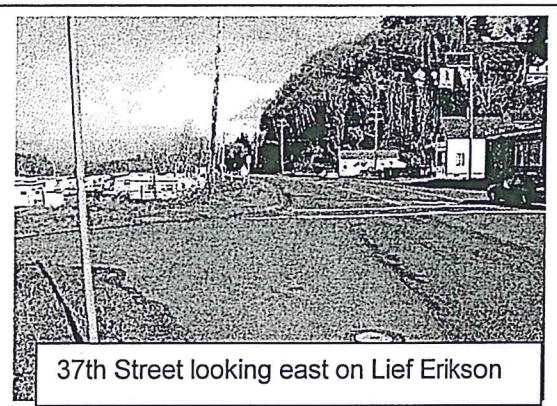
The intersection at 37th Street and Lief Erikson Drive has been the subject of discussions over the years relative to traffic. Lief Erikson Drive curves to the south both to the east and west of this intersection. As a result, the visibility

from the south side of the intersection is more limited than the visibility from the north side. Vehicles exiting 37th Street from the proposed site have good visibility in excess of 800' in both directions of Lief Erikson Drive. Likewise, traffic on Lief Erikson Drive entering 37th Street from either direction have good visibility of on-coming traffic in excess of 800'. There is vegetation on the northwest corner of 37th Street which could impair visibility if allowed to grow and expand. The applicant should keep the vegetation on that corner cut to maintain visibility to the west (Condition 2).

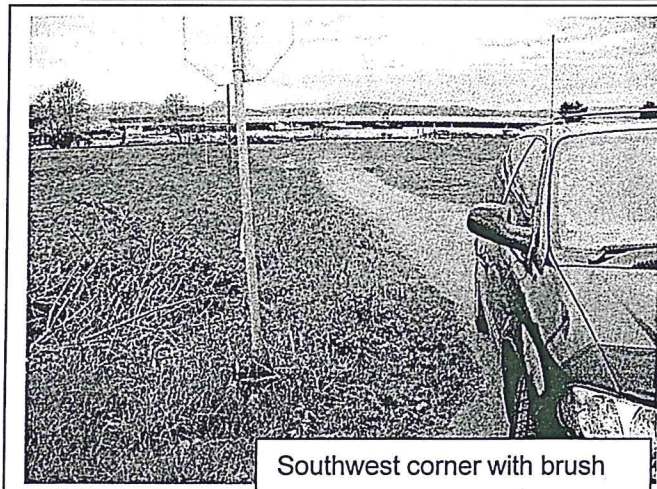
Since the original business opened in 2003, the City has not received any complaints concerning traffic related to this business.



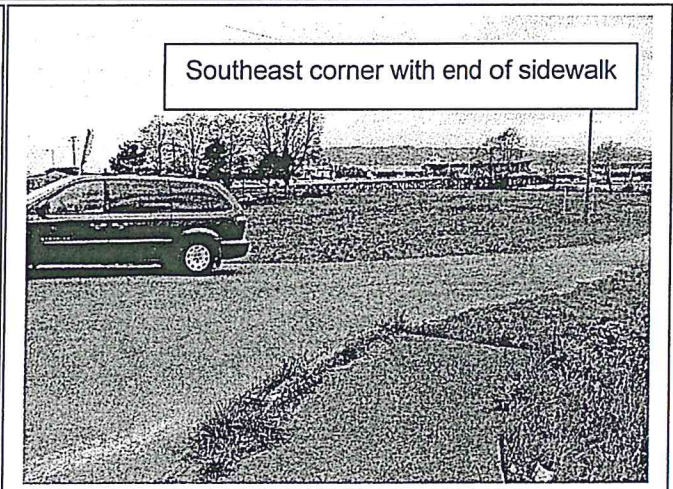
37th Street looking west on Lief Erikson



37th Street looking east on Lief Erikson

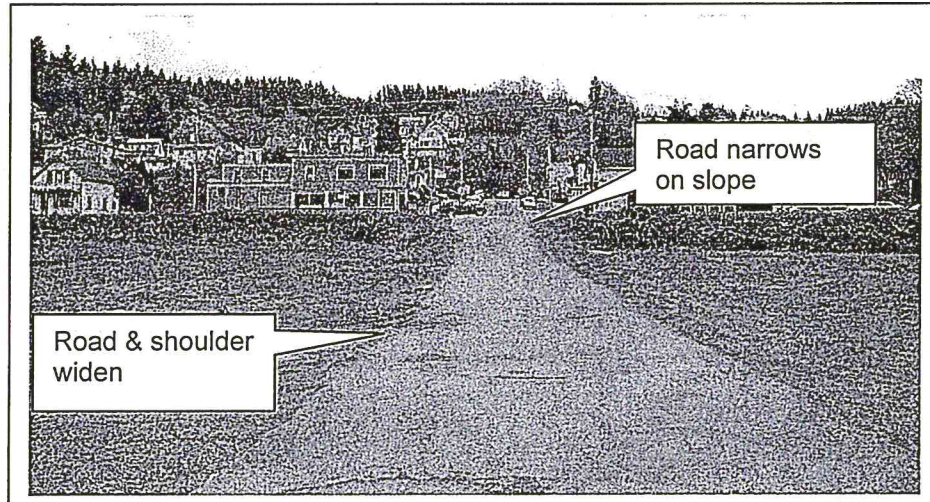


Southwest corner with brush



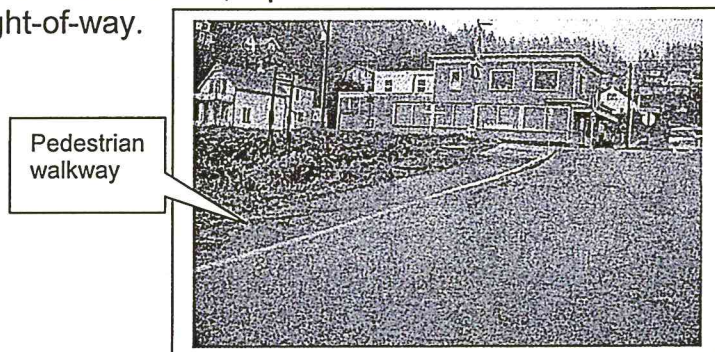
Southeast corner with end of sidewalk

The apron of 37th Street is paved to a width of 40' reducing down to 20' that would allow two vehicles to enter and exit the street at the same time. However, the paved area narrows to 18' wide as it lowers to the level of the proposed site from Lief Erikson Drive and then the unpaved area widens to allow vehicles to pass on the unpaved shoulders. There is ample room for the volume of traffic generated by this operation based on the previous owner's operation. However, some of this changed as the City and ODOT constructed intersection improvements at 37th Street and Lief Erikson Drive in the last few years. These improvements are beneficial to the existing espresso operation.



The facility will have a trash receptacle. There is ample parking available for employees, customers, and maneuvering of vehicles. The nature of the drive-through establishment will limit the number of customers that actually stop at this location.

The 37th Street right-of-way is paved to a width of approximately 18' with no curbs or sidewalks. This street is used by residents of Uppertown for access to the River Trail and Riverfront. The use is proposed as a temporary use which is for minimal capital improvement projects. Access to the site will be on a sub-standard roadway. City standards for a minor local street would be 28' wide with a 5' sidewalk on one side. The existing roadway has some shoulder width in addition to the 18' paved area, but would not support a standard width roadway without the addition of fill. Due to the temporary nature of the use, it would not be feasible to make road improvements at this time. ODOT made improvements to the intersection which included improvements for a pedestrian path from Lief Erikson Drive to the City River Trail. In addition, a pedestrian trail area was marked within the 37th Street right-of-way.



The original permit (CU03-01) contained a condition that "Prior to any permit extension, the APC shall review the need for the applicant or property owner to improve the street width to 28' and the need to install a pedestrian sidewalk on 37th Street north from Lief Erikson Drive." There have been no complaints or incidents reported to the Community Development Department relevant to this business and the need for street

or sidewalk improvements. As noted above, the City and ODOT did construct improvements at the intersection. That condition is no longer needed.

3. Section 11.030(A)(3) requires that *"the use will not overburden water and sewer facilities, storm drainage, fire and police protection, or other utilities."*

Finding: All utilities are available in the area and are capable of serving the use. The use will not overburden the utilities and services.

4. Section 11.030(A)(4) requires that *"the topography, soils and other physical characteristics of the site are adequate for the use. Where determined by the City Engineer, an engineering or geologic study by a qualified individual may be required prior to construction."*

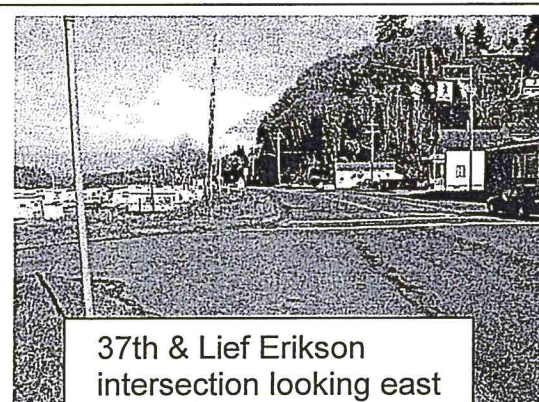
Finding: The subject site is flat, graveled, and no new permanent construction is proposed.

5. Section 11.030(A)(5) requires that *"the use contains an appropriate amount of landscaping, buffers, setbacks, berms or other separation from adjacent uses."*

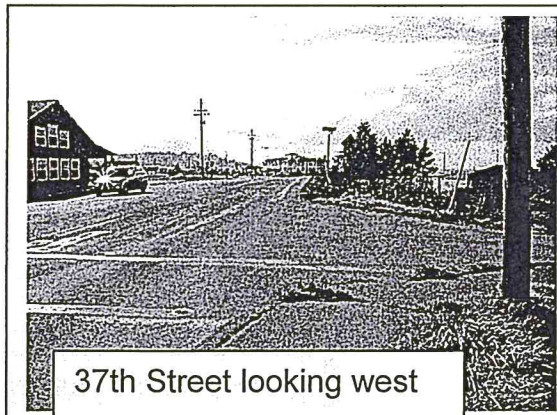
Finding: The site is adjacent to the River Trail and is buffered from other adjacent uses by distance and some vegetation. There are trees separating the site from the animal clinic (former MTC site) to the west, and a fence separates the site from the former trailer court. There are planter boxes on the site. The proposed location is approximately 330' from other uses along the south side of Lief Erikson Drive. The development along the north side of Lief Erikson Drive is changing. The former trailer court to the east is built from the railroad line to the Lief Erikson Drive right-of-way. The animal clinic building on 36th Street is located approximately 70' from Lief Erikson Drive about at mid-block toward the River. At 39th Street, buildings are at various distances from the highway. Two Columbia Loft Condominiums have been constructed, Pier 39 has expanded its operation out over the River, and the Hampton Inn Hotel has been construction with landscaping. Across Lief Erikson Drive, Geno's and the Astoria Coffee Company buildings are located close to the sidewalk along the right-of-way. Other buildings to the east are further back and to the west are open tennis court and ball fields. Buildings in this area are spread out and not compact. The location and siting of the proposed building meets this criteria.



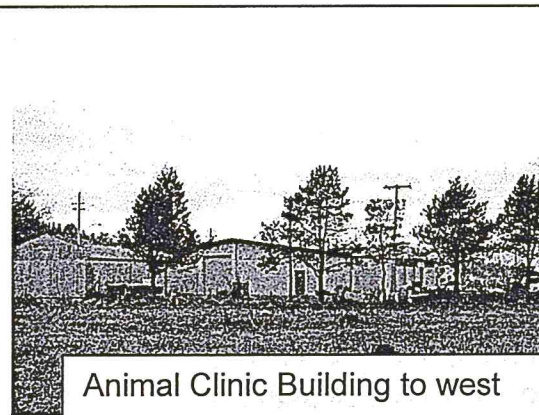
Former Trailer Court to east



37th & Lief Erikson
intersection looking east



37th Street looking west



Animal Clinic Building to west



Hampton Inn at 39th
Street looking
northwest

V. CONCLUSION AND RECOMMENDATION

The request, in balance, meets all the applicable review criteria. Staff recommends approval of the permit with the following conditions:

1. The permit is valid for one year. Prior to expiration, the applicant may request an extension. The Planning Commission, at its discretion, may extend authorization for an additional period up to one year at a time.
2. The vegetation on the northwest corner shall be kept trimmed to not block visibility.

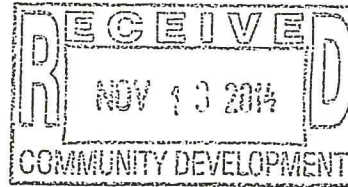
3. Signage shall be in compliance with the provisions of the Sign Code.
4. Significant changes or modifications to the proposed plans as described in this Staff Report shall be reviewed by the Astoria Planning Commission.

The applicant should be aware of the following requirements:

The applicant shall obtain all necessary City and building permits prior to the start of construction.



CITY OF ASTORIA
Founded 1811 • Incorporated 1856
COMMUNITY DEVELOPMENT



No. CU 14-14

Fee: \$250.00

cash

CONDITIONAL USE APPLICATION

Property Address: 230-37th
Lot 1 & 8 E 25' L 2 & 7 Block 1 Subdivision Astoria Port of Upper Ast.
Map 9AC Tax Lot 100 Zone S-1 *OK*
Applicant Name: Michelle Green
Mailing Address: 93079 Scandinavian Rd. Astoria, OR 97103
Phone: 503-325-9513 Business Phone: 503-298-9653 Email: greensmich@msn.com
Property Owner's Name: Port of Astoria

Mailing Address: _____

Business Name (if applicable): _____

Signature of Applicant: Michelle Green Date: 11-13-14

Signature of Property Owner: _____ Date: _____

Existing Use: Temp drive thru coffee shop - vacant

Proposed Use: Temp drive thru coffee shop.

Square Footage of Building/Site: Existing

Proposed Off-Street Parking Spaces: OK
Continue use of the existing Drive-through coffee shop as temporary use for one year.

SITE PLAN: A Site Plan depicting property lines and the location of all existing and proposed structures, parking, landscaping, and/or signs is required. The Plan must include distances to all property lines and dimensions of all structures, parking areas, and/or signs. Scaled free-hand drawings are acceptable.

For office use only:

| | | | |
|-----------------------|-----------------|-----------------------------|-----------------|
| Application Complete: | | Permit Info Into D-Base: | <u>11/17/14</u> |
| Labels Prepared: | <u>11/17/14</u> | Tentative APC Meeting Date: | <u>12/17/14</u> |
| 120 Days: | | | |

FILING INFORMATION: Planning Commission meets on the fourth Tuesday of each month. Completed applications must be received by the 13th of the month to be on the next month's agenda. A Pre-Application meeting with the Planner is required prior to acceptance of the application as complete. Only complete applications will be scheduled on the agenda. Your attendance at the Planning Commission meeting is recommended.

Briefly address each of the following criteria: Use additional sheets if necessary.

- 11.030(A)(1) The use is appropriate at the proposed location. Several factors which should be considered in determining whether or not the use is appropriate include: accessibility for users (such as customers and employees); availability of similar existing uses; availability of other appropriately zoned sites; and the desirability of other suitably zoned sites for the use.

Easy access from Lief Erikson via 37th; close to motel, businesses & River Trail, & East Morning Basin
Drive thru not allowed in most other zones

- 11.030(A)(2) An adequate site layout will be used for transportation activities. Consideration should be given to the suitability of any access points, on-site drives, parking, loading and unloading areas, refuse collection and disposal points, sidewalks, bike paths, or other transportation facilities. Suitability, in part, should be determined by the potential impact of these facilities on safety, traffic flow and control, and emergency vehicle movements.

Large parking area for Port; enclosed trash area; no direct access from State Hwy - access from local street

- 11.030(A)(3) The use will not overburden water and sewer facilities, storm drainage, fire and police protection, or other utilities.

Existing - no change

- 11.030(A)(4) The topography, soils, and other physical characteristics of the site are appropriate for the use. Where determined by the City Engineer, an engineering or geologic study by a qualified individual may be required prior to construction.

Existing - no change

- 11.030(A)(5) The use contains an appropriate amount of landscaping, buffers, setbacks, berms or other separation from adjacent uses.

Existing - no change

- 11.030(B) Housing developments will comply only with standards 2, 3, and 4 above.

Memorandum

Date: December 11, 2014
To: City of Astoria Planning Commission
cc: Brett Estes and Rosemary Johnson, City of Astoria Community Development Department
From: Matt Hastie and Shayna Rehberg
Re: Draft Bridge Vista Area Amendments #1C (Task 8.1) ¹

A. INTRODUCTION/OVERVIEW

In 2009 the City of Astoria adopted the Astoria Riverfront Vision Plan. The Riverfront Vision Plan describes a future vision and specific recommended implementation measures related to open space, land use, and transportation plans along the Columbia River waterfront. For purposes of the Riverfront Vision Plan, City's riverfront was divided into four plan areas: Bridge Vista, Urban Core, Civic Greenway, and Neighborhood Greenway.

In 2012-2013, the City of Astoria requested and received a Transportation and Growth Management (TGM) Code Assistance grant to develop and write updated comprehensive plan language, development code text, and map amendments to implement policies and recommendations in the City's adopted Riverfront Vision Plan for the Civic Greenway area (Phase 1) and Bridge Vista area (Phase 2). Phase 1 has been completed and this memorandum is a part of Phase 2, which addresses the Bridge Vista area, shown in Figure 1. The current zoning within the Bridge Vista area is shown in Figure 2.

In preparation for Phase 2, the project consultants reviewed Comprehensive Plan and Development Code implementation issues identified in the Riverfront Vision Plan for the Bridge Vista area with City staff. Riverfront Vision Plan goals and objectives related to land use in the Bridge Vista area include the following:

- Continue to support water-dependent uses within this area, but allow for a mix of commercial and residential uses that support but don't compete with the Downtown core.
- If development is to occur, promote new uses that are consistent with Astoria's "working waterfront."
- Encourage design of new or rehabilitated buildings that respect Astoria's character.
- Encourage new development along the Columbia River to improve and celebrate the River Trail and provide visual and periodic physical access to the water.

¹ This project is partially funded by a grant from the Transportation and Growth Management (TGM) Program, a joint program of the Oregon Department of Transportation and the Oregon Department of Land Conservation and Development. This TGM grant is financed, in part, by federal Moving Ahead for Progress in the 21st Century (MAP-21), local government, and State of Oregon funds. The contents of this document do not necessarily reflect views or policies of the State of Oregon.

- Improve physical connections to adjacent neighborhoods.
- Use setbacks, stepbacks and other measures to ensure an open feel and continued visual access to the river.
- Work with property owners, including those with existing leases to maximize open areas over the water.
- Change zoning of area west of 2nd Street from Tourist Commercial to other commercial zone.
- Expand (Uniontown) design overlay for the historic district to accentuate the historic area (north of US 30) and create a more prominent gateway for the urban core.

The Vision Plan also notes that “This area is an appropriate location for new overwater development, should it occur. However, specific areas should remain open to preserve broad view of the river.”

This memorandum presents the first draft of the Set C (referred to as **Amendments #1C**) package of recommended policy and code amendments for the Bridge Vista area, the third of three sets of amendments described in the Draft Bridge Vista Area Amendments #1A Memorandum, dated October 27, 2014. The amendments in this memorandum are organized as follows:

- Design Guidelines and Standards
 - Use façade improvement guidelines that the City has drafted to be used for grants in the plan area as a base for potential design guidelines and standards in the area
 - Also draw on the design guidelines adopted for the Gateway Overlay Zone.
 - Structure proposed design guidelines and standards similar to the structure for the residential design guidelines and standards developed for the Civic Greenway area.
- Setbacks
 - Establish minimum and maximum setbacks from public rights-of-way in the Bridge Vista area, with smaller maximum setbacks for yards fronting West Marine Drive.
- Landscaping
 - Use standards for the land side landscaping from the Civic Greenway Overlay Zone as a basis, with minor modifications
 - Use revised landscaping provisions from the Civic Greenway area that better reflect conditions and objectives for landscaping in the Bridge Vista area
- Off-Street Parking
 - Consider reducing off-street parking standards, particularly in the pedestrian-oriented zone

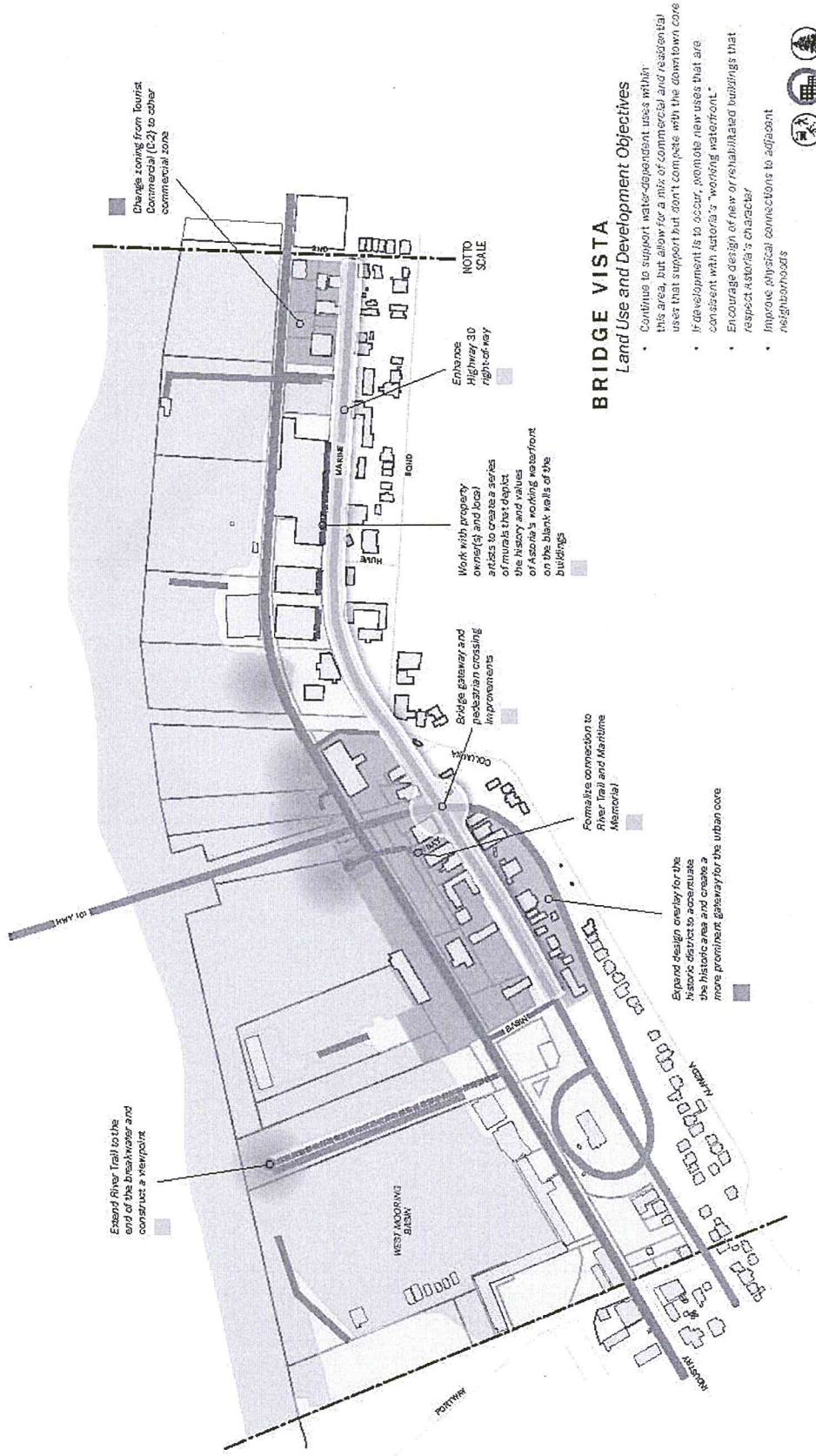
As noted in the outline above, the Draft Astor-West Urban Renewal District Design Guidelines (November 2013), Gateway Overlay Zone, and Civic Greenway Overlay Zone were primary resources in preparing the proposed design guideline and standard concepts and development standard recommendations presented in this memorandum. Consultation with the Project Management Team (PMT)/Project Advisory Team (PAT) at a meeting held on November 25, 2014 was also key in forming these preliminary concepts and recommendations. Other communities’ development codes and State model development code served as secondary resources.

In the first section of this memorandum, the project team has prepared lists of potential design guidelines and standards concepts by design element that the team is seeking focused Planning

Commission review of and comment on. The other sections of the memorandum address development standards, in which the project team has made recommendations about proposed standards and options. Once the Planning Commission and other community members review and comment on these recommendations, they will be revised as needed and presented as adoption-ready code language. The code language can be readily prepared as most of the recommendations in this memorandum refer to and rely on existing code language, with a few exceptions.

DRAFT

Figure 1: Bridge Vista Area



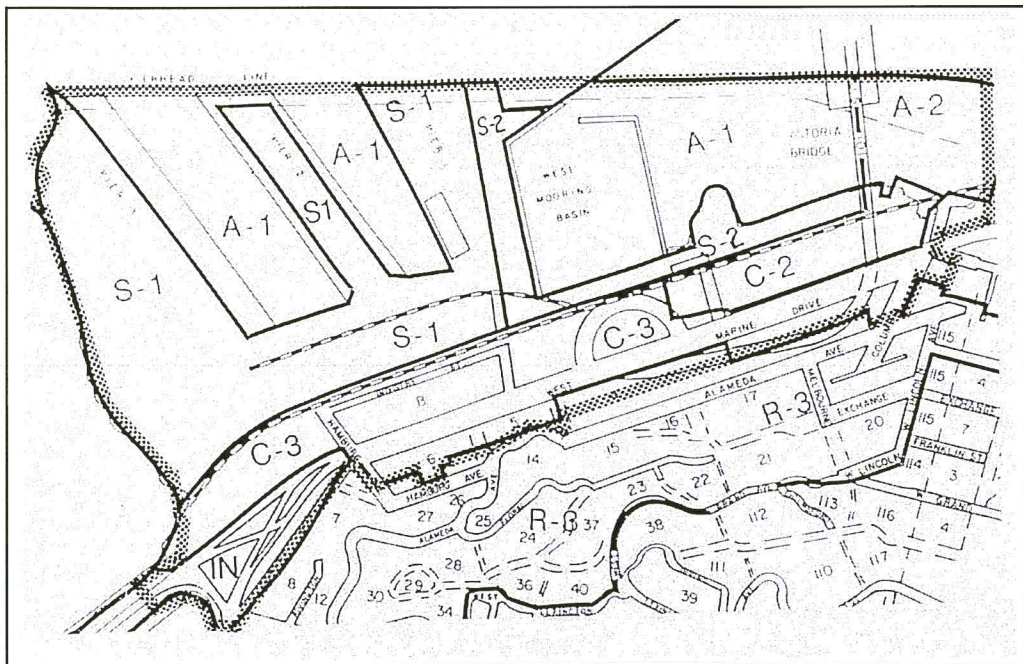
ASTORIA RIVERFRONT VISION PLAN
LAND USE AND URBAN DESIGN IDEAS
OCTOBER 2008

BRIDGE VISTA

B. DESIGN GUIDELINES AND STANDARDS

The City of Astoria Development Commission approved a Storefront and Façade Grant and Loan Program for businesses in the Astor-West Urban Renewal District in 2013. The purpose is to improve the appearance and attractiveness of the exterior facades of properties in the Astor-West Urban Renewal District for the purposes of revitalizing the area, increasing its visual appeal, and creating a healthy climate for small business. The Urban Renewal District is located in western Astoria, extending generally from Columbia Avenue to Smith Point and including property with frontage on West Marine Drive as well as land and aquatic areas north of West Marine Drive, overlapping with the Bridge Vista area to the West Mooring Basin and north of West Marine Drive.

Figure 3: Astor-West Urban Renewal District



Historically, the district has been a primarily working waterfront and design guidelines developed for the grant and loan program were intended to reflect the industrial working waterfront atmosphere. They address historic design and development, building characteristics, lighting, signage, and pedestrian orientation and circulation, with a focus on commercial uses in the district. The City developed the guidelines largely drawing from design guidelines it had prepared and applied in the Gateway Overlay Zone.

Since the program is aimed at renovation of existing buildings, the design guidelines do not address new development. However, the guidelines state that new construction should respect the unique qualities of the existing historic and working waterfront buildings in the area.

The following subsections present proposed guidelines and standards concepts related to the design elements below.

- Building style/form
- Roof forms and materials
- Doors
- Windows
- Siding and wall treatment
- Awnings
- Lighting
- Signs

The general source of each proposed guideline or standard concept is indicated in parentheses following the concept. Sample images are provided for some of the concepts; more images can be provided as requested or needed in a subsequent revised draft of the proposed guidelines. In some cases, questions for the City and/or Planning Commission are included in the list of proposed guidelines and standards concepts in call-out boxes. For some of the design elements, there is a question of whether to discourage (guideline) or prohibit (standard) specified design element types or treatments. Similarly in some cases, desirable treatments could be encouraged or required (should versus shall).

1. Applicability

Design guidelines and standards that are developed from discussion of the following sections are intended to apply to new development and redevelopment in the Bridge Vista area. Redevelopment or “major renovation” can be defined in the Bridge Vista area in a combination of ways as it is in other sections of existing Astoria code:

- Gateway Overlay Zone and Civic Greenway Overlay Zone – Construction valued at 25% or more of the assessed value of the existing structure.
- Landscaping Requirements/Plan – Remodeling with a value of at least 33% of the assessed value of the structure, or in the event of a change of use or installation of new parking areas.

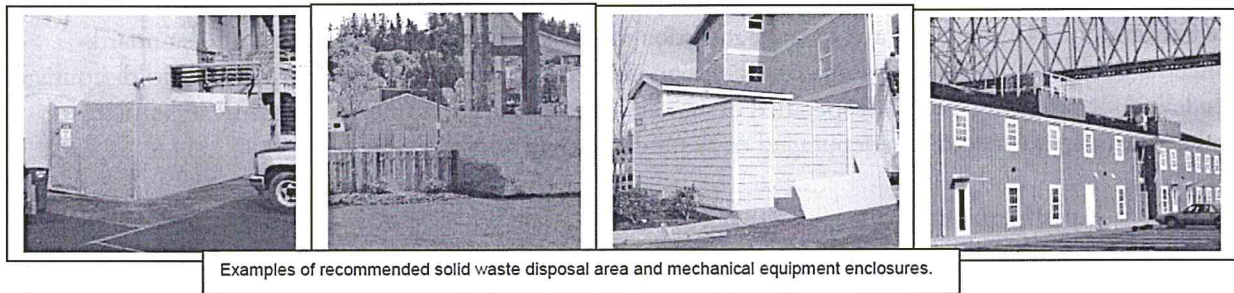
2. Building Style/Form

The building style and form guidelines for the Urban Renewal District are primarily only applicable to building remodeling or renovation. Therefore, the building style and form guidelines concepts below also draw from the Gateway Overlay Zone guidelines, which provide more guidance for new construction.

- Buildings should retain significant original characteristics of scale, massing, and building material along street facades. (Astor-West Urban Renewal District guidelines)
- Additions to buildings should not deform or adversely affect the composition of the facade or be out of scale with the building. (Astor-West Urban Renewal District guidelines)

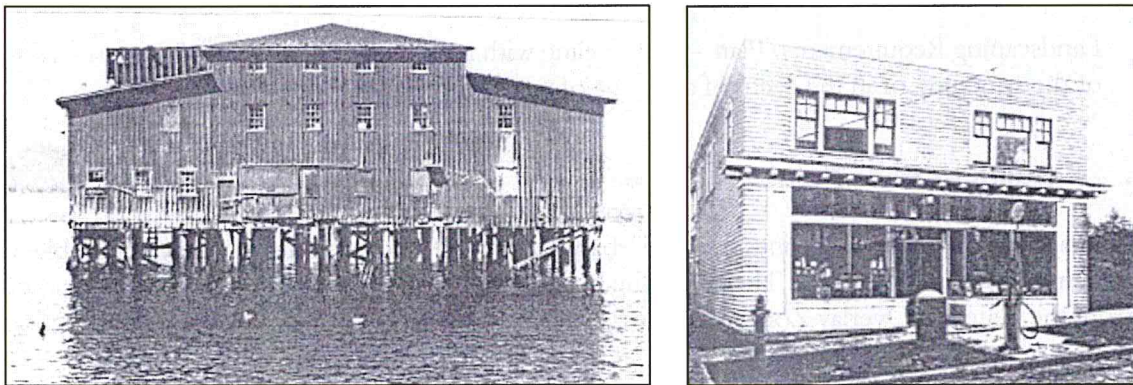
- Distinctive stylistic features or examples of skilled craftsmanship should be treated with sensitivity. All buildings should be respected and recognized as products of their time. (Astor-West Urban Renewal District guidelines and Historic Properties Ordinance standards)
- Mid-century “slip covers” should be removed when possible. (Astor-West Urban Renewal District guidelines)
- Solid waste disposal, outdoor storage, and utility and mechanical equipment should be enclosed and screened from view (Figure 4). Rooftop equipment should be screened from view by a parapet wall, a screen made of a primary exterior finish building material used elsewhere on the building, or by a setback such that it is not visible from adjacent properties and rights-of-way up to approximately 100 feet away. (Astor-West Urban Renewal District guidelines; PMT/PAT consultation; City of St. Helens Development code)

Figure 4: Screening Waste Disposal, Outdoor Storage, and Utility and Mechanical Equipment



- Building forms should be simple single geometric shapes, e.g. square, rectangular, triangular (Figure 5). (Gateway Overlay Zone design guidelines)

Figure 5: Geometric Building Forms [To be updated with photos from the Uniontown area]



- Incompatible additions or building alterations using contemporary materials, forms, or colors on building facades are discouraged. (Astor-West Urban Renewal District guidelines)

- Projecting wall-mounted mechanical units are prohibited or discouraged where they are visible from a public right-of-way. (Astor-West Urban Renewal District guidelines) Projecting wall-mounted mechanical units are allowed where they are not visible from a public right-of-way.

3. Roof Forms and Materials

The Urban Renewal District design guidelines address general building style and form, but not roof forms and materials in particular. Therefore, the concepts for roof form and materials guidelines below draw from the Gateway Overlay Zone guidelines, which provide guidance according to building type.

- The following roof forms are encouraged for industrial buildings (Gateway Overlay Zone design guidelines):
 - Single gable with low pitch
 - Repetitive gable with steep pitch (Figure 6 and Figure 7)
 - Shallow eaves (Figure 7)
 - Small shed roof dormers
 - Monitor roof on ridge line (Figure 7)
 - Flat panel skylights or roof window

Figure 6: Roof Pitches

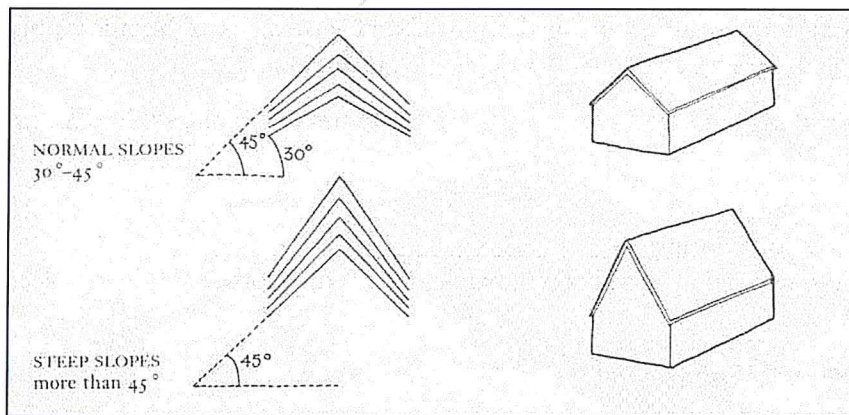
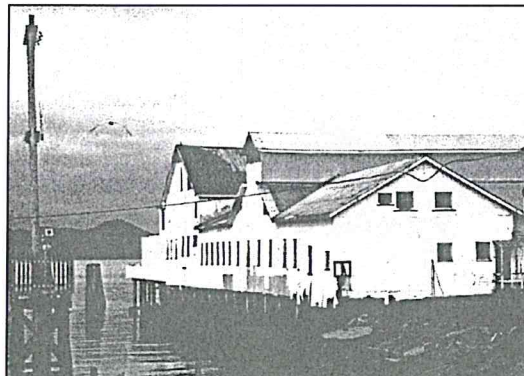


Figure 7: Industrial Building, Multiple Gables, Monitor Roof, and Shallow Eaves



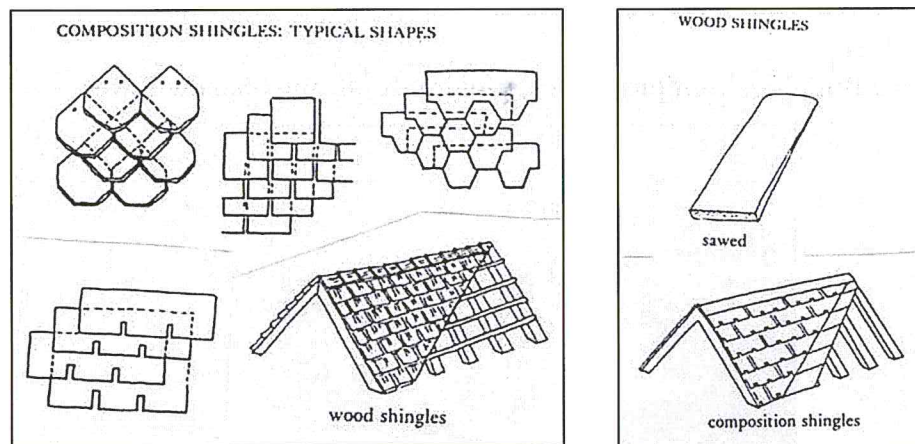
- The following roof forms are encouraged for commercial buildings (Gateway Overlay Zone design guidelines):
 - Single gable with low pitch
 - Repetitive gable with steep pitch
 - Shallow eaves behind parapet wall
 - Flat or gable roof behind parapet wall (Figure 8)
 - Structural skylights

Figure 8: Commercial Building, Flat Roof Behind Parapet Wall



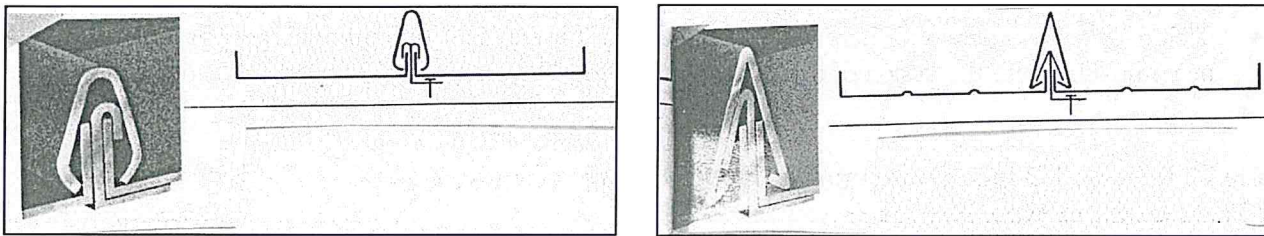
- The following roof forms are discouraged or prohibited: false mansard or other applied forms; and dome skylights. (Gateway Overlay Zone design guidelines)
- The following roofing materials are encouraged for all types of buildings (Gateway Overlay Zone design guidelines):
 - Cedar shingle
 - Composition roofing
 - Roofing material in gray, brown, black, deep red, or other subdued color

Figure 9: Encouraged Roofing Materials



- The following roofing materials are encouraged for industrial buildings (Gateway Overlay Zone design guidelines):
 - Galvanized corrugated metal
 - Low profile standing seam, metal roof (Figure 10)
 - Roll down
- Built-up roofing materials are encouraged for commercial buildings. (Gateway Overlay Zone design guidelines)

Figure 10: Low (3/8" x 1") and High (1/4" x 1-1/4") Roof Seams



- The following roofing materials are discouraged or prohibited for all types of buildings: high profile standing seam metal roof (Figure 10); and brightly colored roofing material. (Gateway Overlay Zone design guidelines)

4. Doors

The PMT/PAT advised that the Urban Renewal District guidelines serve as the basis for the door design guidelines in the Bridge Vista area. Door design guidelines concepts are presented below.

- Doors should remain recessed when feasible (Figures 11 and 12). (Astor-West Urban Renewal District guidelines)
- Large cafe or restaurant doors that open the street to the interior by pivoting, sliding, or rolling up overhead are encouraged (Figure 11). (Astor-West Urban Renewal District guidelines)

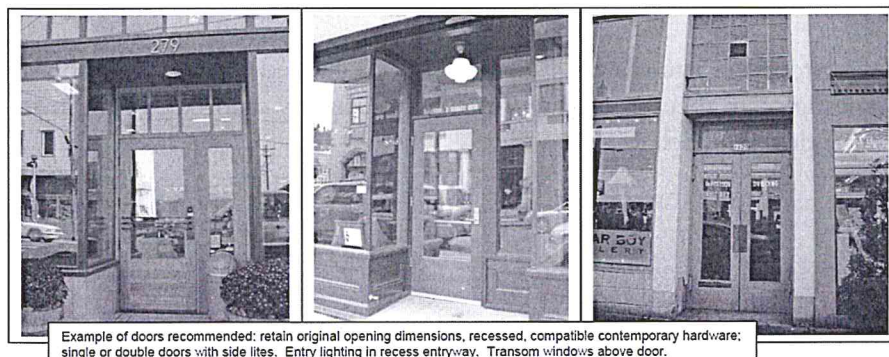
Figure 11: Roll-Up Doors and Recessed Doors



Examples of doors recommended: roll up doors to create open space into the buildings, recessed door

- Doors with a minimum of 50% of the door area that is glass are encouraged. (Astor-West Urban Renewal District guidelines)
- Building lighting should emphasize entrances. (Astor-West Urban Renewal District guidelines)
- Transom, side-lites, or other door/window combinations are encouraged (Figure 12). (Astor-West Urban Renewal District guidelines)
- Doors combined with special architectural detailing are encouraged. (Astor-West Urban Renewal District guidelines)
- Double or multiple door entries are encouraged (Figure 12). (Astor-West Urban Renewal District guidelines)
- Well-detailed or ornate door hardware is encouraged (Figure 12). Contemporary hardware should be compatible with the design of the door. (Astor-West Urban Renewal District guidelines)

Figure 12: Recessed Doors, Contemporary Door Hardware, Single/Double Doors, Side Lites, and Transom Windows



- The following types of doors and door treatments are discouraged or prohibited: solid metal or wood doors with small or no windows; automatic sliding doors; primary entry doors raised more than three feet above sidewalk level; doors flush with building façade; clear anodized aluminum frames; and reflective, opaque, or tinted glazing. (Astor-West Urban Renewal District guidelines)

5. Windows

The Urban Renewal District guidelines discourage blank walls, and the PMT/PAT advised that the guideline be expanded to differentiate between commercial and industrial buildings and between ground floors and upper floors, as well as to establish a standard for the pedestrian-oriented zone of the Bridge Vista area, where window coverage is an important part of pedestrian orientation. Otherwise, the team advised that the Urban Renewal District guidelines be used as a starting point for window design in the Bridge Vista area. The team seeks more extensive Planning Commission review and comment on this set of design guidelines and standards concepts.

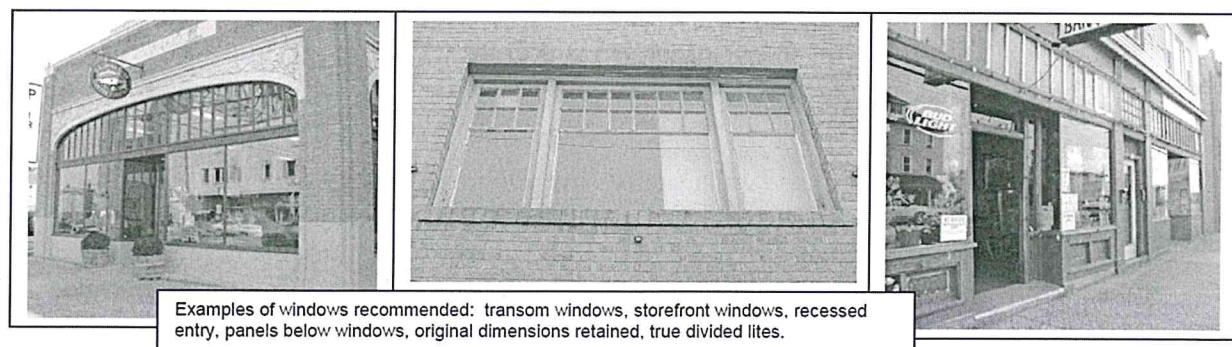
- All building facades visible from a public right-of-way shall have windows or other openings in the facade. (Astor-West Urban Renewal District guidelines; PMT/PAT consultation)

- In the pedestrian-oriented district, at least 50% of the ground-floor street-facing facades of non-industrial uses shall include windows and at least 30% of the upper-floor street-facing facades should include windows. (PMT/PAT consultation; Model Development Code for Small Cities)
- All building facades of industrial buildings in the Bridge Vista area that are visible from a public right-of-way shall have windows, but industrial buildings are not subject to minimum window area requirements. (PMT/PAT consultation) Industrial and commercial buildings will be defined in the next draft of these standards and guidelines.
- Clear story and transom windows are encouraged for the upper story facades of industrial buildings. (PMT/PAT consultation)
- Blank walls on any facades visible from the street for any type of use are prohibited.

Question for the Planning Commission: Do you want to set minimum but lower glazing standards for commercial buildings outside of the pedestrian-oriented zone in the Bridge Vista area?

- Windows, including transoms on existing buildings, should retain their original size and location as part of renovation activities. (Astor-West Urban Renewal District guidelines)
- Windows that open by pivoting, casement, single hung, or other shuttering are encouraged. (Astor-West Urban Renewal District guidelines)
- Painted wood or stucco panels or tile clad panels below windows are encouraged (Figure 13). (Astor-West Urban Renewal District guidelines)
- Clear glass is encouraged. (Astor-West Urban Renewal District guidelines)
- True divided lites are encouraged or required (Figure 13). (Astor-West Urban Renewal District guidelines). Simulated divided lites shall have exterior muntins to create exterior shadow lines.
- Boldly articulated window and storefront trim are encouraged. (Astor-West Urban Renewal District guidelines)

Figure 13: Transom Windows, Panels Below Windows, True Divided Lites



- The following types of windows or window treatments are discouraged or prohibited: residential-styled window bays on commercial buildings; half-round windows; tinted and/or reflective glass;

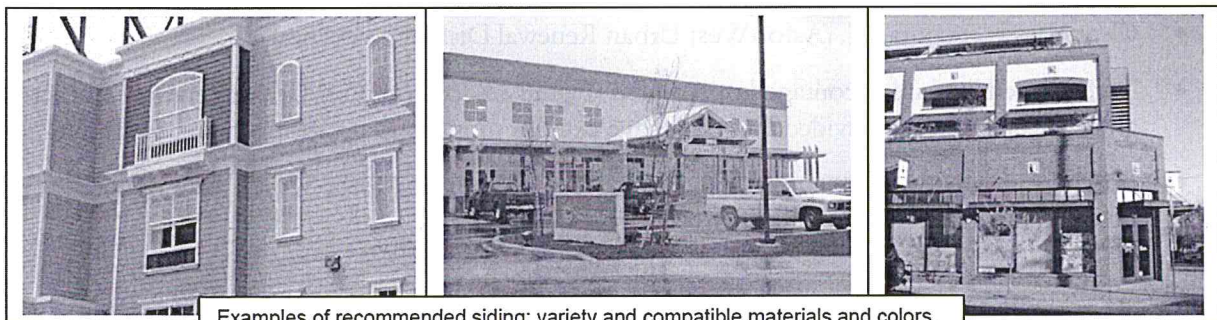
simulated divided lites; sliding windows; vinyl windows; and blocked out windows. (Astor-West Urban Renewal District guidelines)

6. Siding and Wall Treatment

The PMT/PAT advised that the Urban Renewal District guidelines serve as the general basis for the proposed design guidelines concepts below.

- Variations in wall cladding materials and patterns consistent with historic patterns are encouraged (Figure 14). (Astor-West Urban Renewal District guidelines)
- Natural or subdued building colors are encouraged (Figure 14). (Astor-West Urban Renewal District guidelines)
- Bright colors may be used for accent trim in limited amounts. (Astor-West Urban Renewal District guidelines)
- Durable materials such as brick, stucco, granite, pre-cast concrete, board and batten, or horizontal wood siding should be used (Figure 14). (Astor-West Urban Renewal District guidelines) These materials include galvanized corrugated metal for industrial buildings. (Gateway Overlay Zone design guidelines; PMT/PAT consultation)

Figure 14: Siding Variety and Compatible Materials and Colors



- Architectural wall features such as belt courses, pilasters, and medallions are encouraged. (Astor-West Urban Renewal District guidelines)
- The following types of siding and wall materials and wall treatments are discouraged or prohibited: cladding materials such as corrugated metal panels or spandrel glass; panels that are poorly detailed or do not have detailing; neon or other fluorescent colors; bright or primary wall colors for the entire wall surface; flagstone, simulated river rock, or other similar veneer cladding; painted brick; or non-durable materials such as synthetic stucco or shingles at the ground floor. (Astor-West Urban Renewal District guidelines)

Question for the Planning Commission: Do you want to further differentiate siding/wall treatment design guidelines for commercial buildings and industrial buildings?

7. Awnings

While awnings can provide weather protection, the PAT/PMT advised that awnings should not be required in the Bridge Vista area and should be limited to protect views of the river in the area. The proposed guidelines and standards concepts below reflect that direction, as well as direction to limit or prohibit types of awnings that are inconsistent with the historic style of the area.

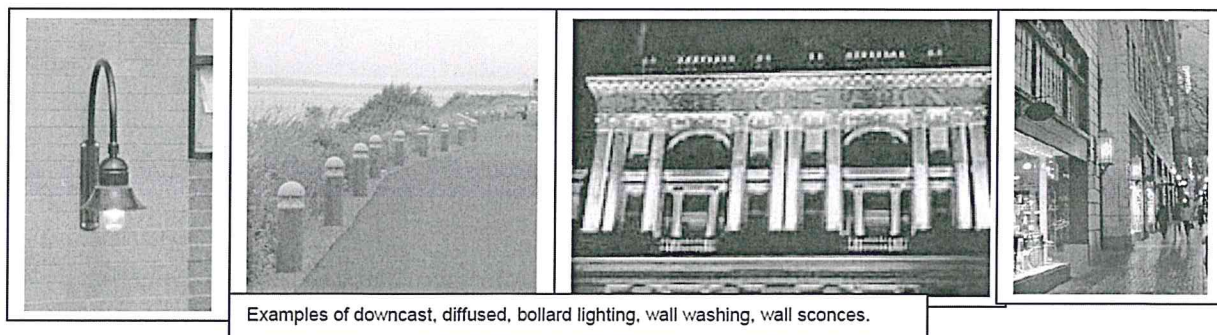
- Awnings and weather protection are permitted in the Bridge Vista area where they do not interfere with views of the river along the River Trail or along north/south streets in the area. In these areas, awnings are generally discouraged and shall not project into the setback area. (PMT/PAT consultation)
- The following types of awnings and awning treatments are discouraged or prohibited: fixed “bubble shaped” awnings; vinyl or other non-compatible material awnings; awnings improperly sized for the building/entry/window; and awnings lit internally. (Astor-West Urban Renewal District guidelines)

8. Lighting

The PMT/PAT advised that some measure of lighting guidance be provided in the Bridge Vista area. The proposed guidelines and standards concepts below are generally based on other lighting provisions developed by the City, with a focus on (a) minimizing the amount of light projecting upward, which is of particular concern for the night sky, or that glares into other properties and/or traffic; and (b) discouraging lighting types that are not consistent with the historic style of the area.

- Outdoor lighting shall be designed and placed so as not to cast glare into adjacent properties. Light fixtures should be designed to direct light downward and minimize the amount of light directed upward, including lighting from wall-washing fixtures. The Community Development Director may require the shielding or removal of such lighting where it is determined that the lighting is adversely affecting adjacent properties or directing significant light into the night sky. (Civic Greenway Overlay Zone)
- Wall-washing lighting fixtures. Fixtures should be concealed and integrated into the design of buildings or landscape walls and stairways (Figure 15). (Astor-West Urban Renewal District guidelines)

Figure 15: Downward and Diffused Lighting, Wall-Washing Lighting



- The following lighting types or treatments are discouraged or prohibited: neon silhouette accent lighting; fluorescent tube lighting; security spotlight; signs lit by lights containing exposed electrical conduit, junction boxes; or other electrical infrastructure; up lighting or light that shines into other properties or traffic.

9. Signs

The City's existing sign provisions should be sufficient to regulate signs in the Bridge Vista area. However, the PMT/PAT advised that some signs be considered for restriction at least in the pedestrian-oriented zone in the Bridge Vista area.

- Prohibit monument signs and freestanding pole-mounted signs in the pedestrian-oriented zone (Figure 16). (Gateway Overlay Zone design guidelines; PMT/PAT consultation)

Figure 16: Monument Signs and Freestanding Pole-Mounted Signs



Question for the Planning Commission: Should monument signs be prohibited in the pedestrian-oriented zone in the Bridge Vista Area if doing so would make some existing signs non-conforming (e.g., the monument signs of two motels)?

C. SETBACKS

Given the objectives of promoting the historic character and strengthening the pedestrian orientation of development in the Bridge Vista area, minimum and maximum setbacks should be considered for yards along public rights-of-way in the area.

1. Minimum Setbacks

Minimum setback requirements in the Bridge Vista area were proposed in the Amendments #1a Memorandum (dated October 27, 2014), which focused on protecting views of the river. These included minimum setbacks along north-south rights-of-way between Marine Drive and the Columbia River that allow for a 70-foot view corridor, as well as 10-foot setbacks on the south side of the River Trail and 20-foot setbacks on the north side of the River Trail. Minimum setback requirements that minimize setback and strengthen the area's pedestrian orientation should be considered for other rights-of-way in the Bridge Vista area, particularly West Marine Drive.

2. Maximum Setbacks

Maximum setbacks can also be established to reinforce historic character and pedestrian orientation in the Bridge Vista area. Smaller maximum setbacks can be instituted along West Marine Drive in order to more strongly focus on pedestrian orientation in that corridor of the Bridge Vista area. Provisions can also be made for extending the maximum setbacks if pedestrian-oriented amenities are provided in that space.

Recommendations:

- Minimum setbacks – The minimum setback for yards fronting West Marine Drive and other public rights-of-way parallel to West Marine Drive in the Bridge Vista area, with the exception of the River Trail, shall be zero (0) feet.
- Maximum setback for the Bridge Vista area – The maximum setback for yards fronting a public right-of-way in the Bridge Vista area, except for West Marine Drive, shall be 10 feet.
- Maximum setback for West Marine Drive – The maximum setback for yards fronting West Marine Drive in the Bridge Vista area shall be five (5) feet.
- Extending the maximum setback – The maximum setback for yards fronting a public right-of-way in the Bridge Vista area may be extended to 20 feet for up to 50% of the building façade if the setback is used for a walkway, plaza, courtyard, or other pedestrian-oriented or public gathering space.

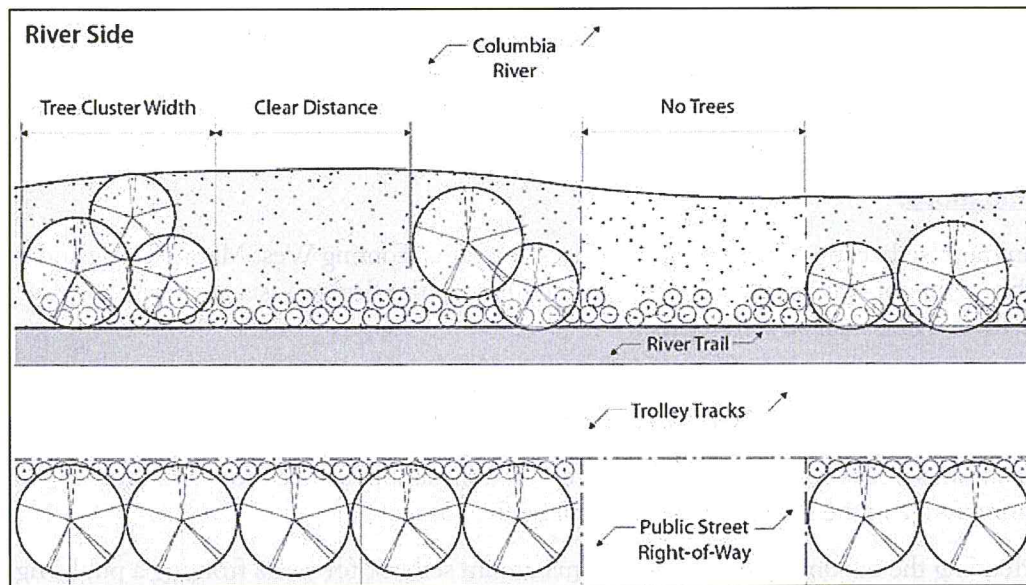
D. LANDSCAPING

A set of landscaping standards was developed for the river side of the River Trail, the land side of the River Trail, and for street trees in the Civic Greenway area. Those standards are reviewed below in order to gather feedback on whether they should be applied in the Bridge Vista area.

1. River Side/Riparian Standards

Standards developed landscaping on the river side of the River Trail in the Civic Greenway area were focused on promoting native and riparian-appropriate planting as well as protecting visual access to the river. The following height and spacing standards were adopted particularly to protect views of the river.

- Maximum shrub height is 30 inches.
- Trees shall not exceed 35 feet in height at maturity
- Maximum width of clusters of trees is 50 feet.
- Clusters of trees shall have a minimum of 50 feet clear between branches at maturity.
- Trees are not permitted to be planted on the river side of the River Trail within the extended public right-of-way or view corridor extending from it for a distance of 70' centered on the right-of-way centerline.
- Maximum height of fences is three (3) feet.

Figure 17: River Side/Riparian Landscaping Diagram

In the Civic Greenway Overlay Zone, landscaping on the river side of the River Trail must consist of native plants that are either part of a list of trees, shrubs, and grasses provided in a section of the overlay zone or are determined to be native plants according to documents cited or recommended by City staff. There are also provisions for the Community Development Director, or designee, to approve plants that are not native under certain conditions.

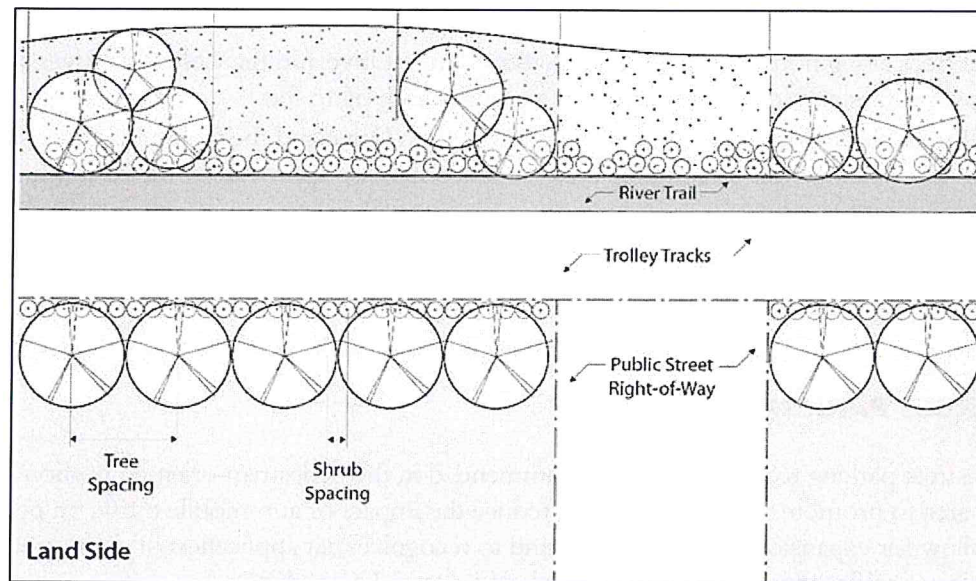
Recommendations:

- Most of the standards from the Civic Greenway area (described above) should be applicable to the Bridge Vista area.
- Tree clusters should be discouraged.
- Hardscaping (courtyards, plazas, etc.) is encouraged and may substitute for or make up a significant percentage of landscaped areas.
- Permeable paving and other stormwater management techniques are encouraged in the design of courtyards and plazas.

2. Land Side/Upland Standards

Objectives for landscaping differed in some ways for the land side of the River Trail in the Civic Greenway area. Denser landscaping was envisioned for this side of the River Trail, so maximum spacing standards and groundcover requirements were developed. At the same time, preserving views was still important, so a maximum height standard was included. The following standards were adopted:

- Maximum spacing of trees is 20 feet on center.
- Maximum spacing of shrubs is five (5) feet on center.
- Ground cover landscaping is required in between shrubs and trees.
- Trees shall not exceed 35 feet in height at maturity.

Figure 18: Land Side/Upland Landscaping Diagram

Other landscaping issues addressed in the Civic Greenway Overlay Zone include parking area landscaping and landscaping credits for non-vegetation features. Existing parking area landscaping standards – that apply between parking areas, streets, and sidewalks – were simply extended to apply between parking areas and the River Trail as well. However, those standards do not address the height or width of this buffer landscaping.

Landscaping credit provisions allow the Community Development Director to approve landscaping credits of 10-25% for non-vegetative features including amenities for the River Trail (e.g., bench, bike rack, drinking fountain, lighting, etc.).

Recommendations:

- Apply the same landscaping provisions developed for the Civic Greenway area with additional standards or guidelines (such as those below).
- Landscaping should minimize pedestrian exposure to parking lots, including visual and physical separation such as a 36" to 42" hedge or an ornate fence.
- Parking area landscaping requirements should specify maximum tree heights and widths (e.g., 15 feet height and width at maturity).

Question for the Planning Commission: Are there other land side/upland landscaping standards or guidelines that should be considered?

3. Street Trees

Similar to the objectives for landscaping on either side of the River Trail, street tree standards were adopted in the Civic Greenway area to help protect visual access to the river. As such, the standards included the following maximum height, width, and associated provisions:

- Maximum height for street trees along north-south streets between Marine Drive and the Columbia River is 45 feet.
- Street trees along north-south streets between Marine Drive and the Columbia River shall have narrow profiles and/or be pruned to a maximum width of 15 feet.
- Street trees along north-south streets between Marine Drive and the Columbia River shall be one of the columnar species listed in the code, unless otherwise approved by the Community Development Director.

Recommendation: Apply the same street tree standards in the Bridge Vista area.

E. OFF-STREET PARKING

Reduced off-street parking requirements are recommended in the pedestrian-oriented portion of the Bridge Vista area to promote pedestrian activity, reduce the impact of automobile traffic on pedestrians, more easily allow for expansion of existing uses, and to recognize that application of existing standards would result in a significant portion of a given lot being covered by parking areas.

Recommendations:

- Reduce existing off-street parking requirements for all new uses in the Bridge Vista area by approximately 50%.
- Waive or further reduce off-street parking requirements for uses smaller than 5,000 square feet in floor area or for existing buildings that cover the entire site.

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| <p>Question for the Planning Commission: Should expansions of existing uses of 40% or less be exempted from off-street parking requirements?</p> |
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November 25, 2014



Astoria Planning Commission

RE: Bridge Vista

Circumstances prevented me from attending last night's meeting and therefore I would like this letter entered into the public record as it relates to the Bridge Vista discussions.

As a lifelong member of the community I've very much appreciated and enjoyed the easterly views of the river and downtown area that we were afforded from our vantage point high on the hill. However from a distance the river was the river and always more abstract: like an ever changing painting.

My perception has changed due to a recent move to Columbia House. The river has gone from something abstract to something very intimate. Its character changes when you're very close to it, and it's the closeness and intimacy that the river walk affords that I feel needs to be maintained at all cost. We really have very few open sweeping vistas along the length of the river walk where you can view not only the bridge but the river itself and experience just how large a body of water it is, and how much river traffic passes through our area. Except for a few key areas along the river walk there are few places that are unencumbered by buildings that allow both pedestrians and vehicles to see larger segments of the rivers geography, and these need to be maintained in each of the four vision plan areas. The primary area in the Bridge Vista section runs from 2nd street to Astoria Warehousing. We have a west view and therefore have the opportunity to watch what happens in this area, and I've been astounded by the number of people who use the river walk, and even more so by the number who linger on the trestle to watch the river, the birds, the seals, and to point and discuss the various rocks, pilings, boiler, etc. that can be found in this section. It's my understanding that there may be a petition to have this area declared an historical site, and if that does occur I would implore you to vote yes as this particular area is treasure trove for visitors to the area due to the artifacts that can be seen when the tide is low, and because of its topography also serves as a small estuary of sorts which is unique to this section.

As it pertains to zoning I fully support maintenance of a working waterfront, and water dependent industry, but question how one could interpret a hotel as a water dependent industry, or include such structures in the term working waterfront. To me that means businesses that need water access to function: fish processing, boat repair, marina, etc., all of which have and have had a historical presence here, and it's quite a stretch to include lodging in that definition. I would caution you to be very prudent in your zoning decisions and not allow those decisions to impact what few sweeping views remain.

As you debate this issue I would encourage each of you to both walk the Bridge View section of the river walk and also drive it in order to get a good first hand perception of what is and what isn't, and to keep in mind that the majority of our historical industries continue to shrink, and that tourism is now an important industry and one that is significant to our local economy. As you take your walk try to view this area as a tourist might: both on foot or on the trolley. What you can see is what adds to Astoria's charm and character, and it is these two factors that have helped propel its tourism. Polluting our view with more structures on the river side will do nothing to enhance the beauty and character, but only detract.

Cordially yours,

Russ Farmer
1 3rd Street, #501
Astoria, OR 97103